

Nondestructive Evaluation of 4 Bridges in La Crosse, WI

submitted to

City of La Crosse
400 La Crosse Street
La Crosse, WI 54601

by

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1. Introduction

The objective of this project is to evaluate the prescribed conditions of four bridges located in the City of La Crosse as detailed in Table 1. The project will utilize nondestructive methods including infrared thermography (IR) for the deck delamination surveys, and ground penetrating radar (GPR) for the abutment wall and approach slab void surveys. The information resulting from the IR and GPR surveys will be used by the City of La Crosse to facilitate subsequent rehabilitation planning efforts.

Table 1 – Bridges and Corresponding Survey Objectives

Feature On	Feature Over	Approx. Deck Survey Area (sf)	Approx. Void* Survey Area (sf)	Survey Objectives
George Street	Railroad	30,492	NA	Quantify and map rebar-level delamination
Lang Drive	River	NA	1,500	Quantify and map voids within soil behind abutment walls
Rose Street	Railroad	10,800	28,800	Quantify and map rebar-level delamination. Quantify and map voids beneath approach pavement adjacent to longitudinal retaining walls
Copeland Avenue	Railroad	9,108	32,040	Quantify and map rebar-level delamination. Quantify and map voids beneath approach pavement adjacent to longitudinal retaining walls

*includes abutment walls for Lang Drive bridge, and approach pavement adjacent to retaining walls for Rose Street and Copeland Avenue bridges

2. Deck Delamination Surveys

2.1 Infrared Thermography (IR) Survey

The deck delamination surveys will utilize infrared thermography (IR), and will be carried out according to ASTM D 4788 – 03 using a high resolution distance triggered infrared camera and a high-resolution video camera (1080p x1920p) operated from an elevated platform attached to a survey vehicle (see Figure 1). The infrared camera is capable of capturing a full lane width of data per driving pass, has a frame rate of 60 frames per second, and thermal sensitivity of less than 0.1 degree C. To facilitate accurate location of defects, both a high precision electronic distance measurement encoder (+/- 3" accuracy) attached to the vehicle wheel and an integrated GPS unit will be collected synchronously with the IR and visual survey systems. All data collection is carried out at normal driving speeds, and no closures are required. In addition to the IR and visual data, information on the environmental conditions including the ambient air temperature and wind speed will be recorded.



Figure 1 - Infrasense Infrared Survey Vehicle

2.2 Infrared Thermography Analysis Procedures

The infrared data will be analyzed to quantify and map deck delamination and/or overlay debonding. The IR data will be reviewed simultaneously with the video data to differentiate delaminated areas from surface features (discoloration, oil stains, sand and rust deposits, etc.) that appear in the infrared, but are unrelated to subsurface conditions. Figure 2 shows an example of delaminated areas as they appear in a single image of infrared data and in the corresponding visual data. Each shows a full lane width, the left image representing the infrared data and the right image representing the visual data. The center foot of each infrared image is attached to the center foot of the following image and so on, so that a single strip image is obtained for each pass. The strip image for each pass is placed next to those of adjacent passes to produce a thermal image of the entire deck (see Figure 3).

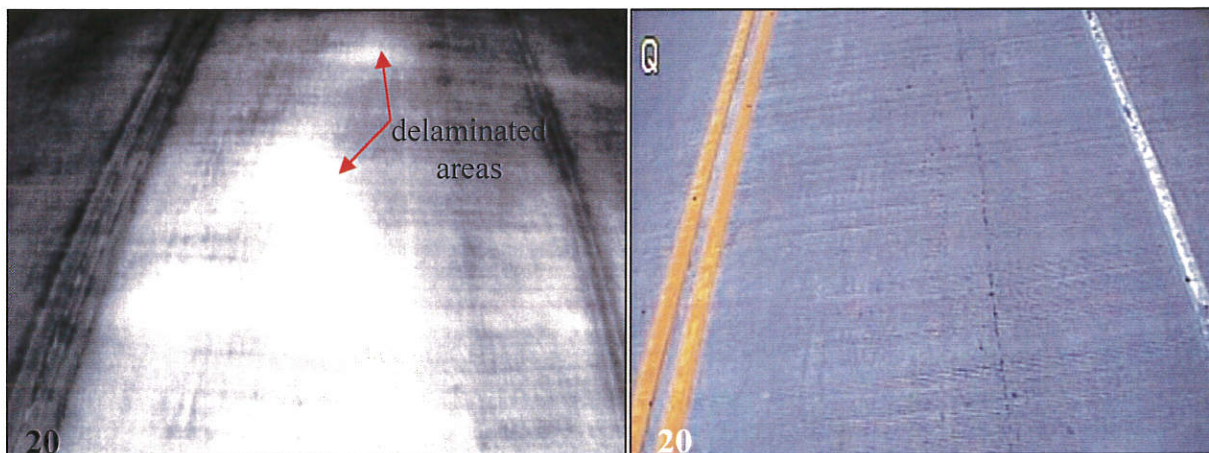


Figure 2 – Sample infrared and visual images at the same location

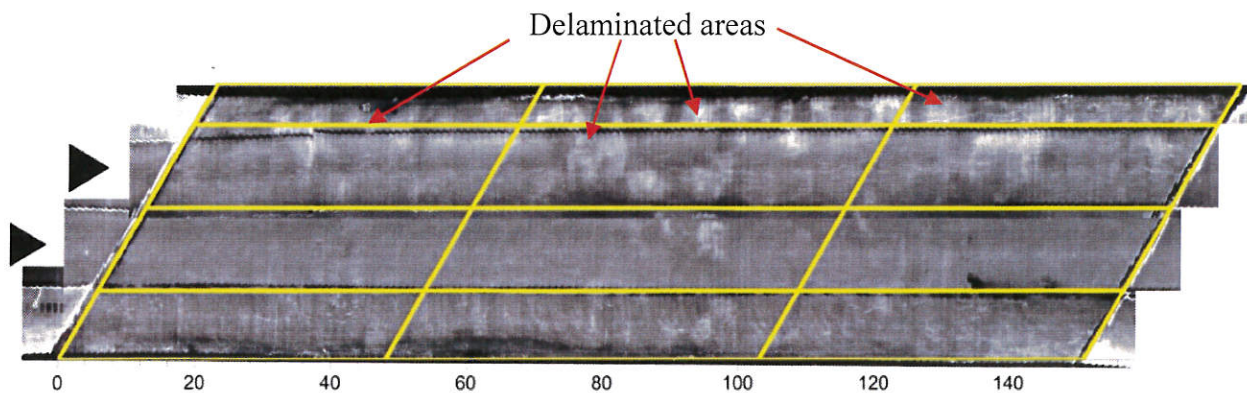


Figure 3 – Sample composite infrared deck image showing delaminations

The white blotchy areas on the IR images shown in Figures 2 and 3 indicate delaminations. These are "hot spots" where the surface temperatures are higher due to the thermal barrier produced by the delaminations. For decks with overlays, these delaminations can be related to debonding of the overlay, or delamination at the rebar level.

A thermal composite image similar to the one shown in Figure 3 will be created for the Copeland Ave., Rose St. and George St. bridge decks. The delaminated areas that appear in the image will be outlined with a cursor. These outlined areas will then be quantified and used to create final plan area maps.

3. Void Surveys (*Behind Abutment Walls and Beneath Approach Pavement*)

3.1 Ground Penetrating Radar (GPR) Survey

The extent of voiding in the soil behind the abutment walls and below the approach slabs will be assessed using ground penetrating radar (GPR). The void quantities and locations will facilitate better understanding the settlement of the parapet walls of the Lang Drive Bridge, and the approach pavement adjacent to the longitudinal retaining walls of the Rose Street and Copeland Avenue bridges.

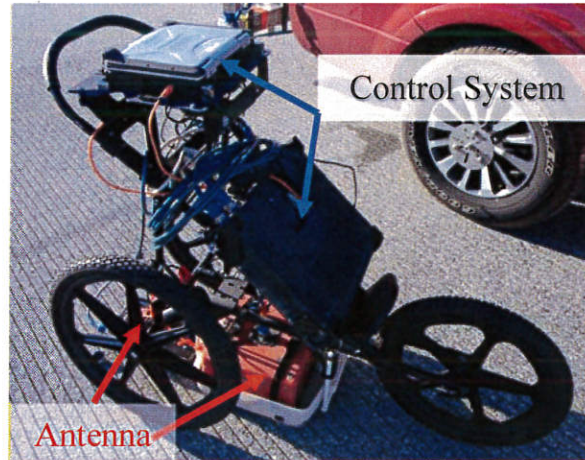


Figure 4 - GPR equipment – 400 and 900-MHz antenna and control system mount to survey cart

The GPR survey will be carried out using a GSSI manufactured survey equipment. The Lang Drive Bridge will be surveyed using a SIR-3000 data collection system coupled with a 900-MHz ground coupled GPR antenna (Figure 4). Data collection along the Copeland Ave. and Rose St. bridges will be completed using a SIR-20 data collection unit paired with dual 1-GHz air coupled antennas (Figure 5).

Each antenna system will be calibrated to site conditions during the time of the survey to provide optimal resolution vs. depth penetration. To ensure accurate location of defects, a high resolution electronic distance measurement encoder (± 3 " accuracy) attached to the antenna systems will trigger the data collection.

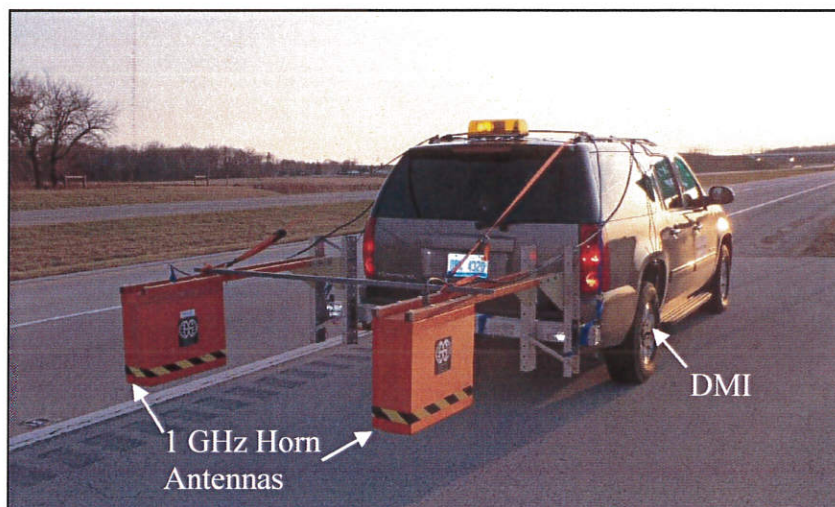


Figure 5 - Infrasense Dual Antenna Survey Vehicle

Lang Drive Survey

The focus of the Lang Drive survey will be the supporting abutment walls and retaining walls along the approach sections (Figure 6). Data collection will be completed without the need for lane closures. A series of parallel traverses spaced no more than 2-feet apart will be collected using a site referenced survey grid to be created at the time of the survey using fiberglass tapes and chalk.



Figure 6 – Evidence of Settlement of Lang Drive Bridge

Rose Street and Copeland Avenue Surveys

Data collection of the Rose Street and Copeland Avenue bridges will focus on the deck surface along the driving lanes and sidewalk in the approach sections (Figure 7). Lane closures will not be required to complete the survey of the Rose Street and Copeland Avenue bridges, as the survey can be completed at traffic speed. The GPR survey will be conducted according to ASTM D 6087 - 08 using twin air-coupled horn antennas suspended above the pavement surface (see Figure 5). Multiple survey lines will be collected using a series of round-trips across the deck, with adjacent data lines spaced transversely at approximately 3 feet apart.



Figure 7 - Rose St. bridge showing the deck surface to be scanned and settlement issue.

3.2 GPR Analysis Procedures

The GPR data will be processed and analyzed using a combination of Infrasense’s proprietary WinDecar software package and GSSI’s Radan processing software to detect areas of anomalous activity that are indicative with voiding, settlement, or loss of material and/or moisture saturation. These conditions appear in the GPR data as high amplitude reflection anomalies as shown in Figure 8. The void analysis uses the amplitude of the “picked” layer associated with the bottom of the approach pavement structure. Amplitude thresholds will be applied to distinguish low, medium, and high probability of voiding/ evidence of moisture settlement. The voiding/ high moisture locations will be quantified and mapped as shown in Figure 9.

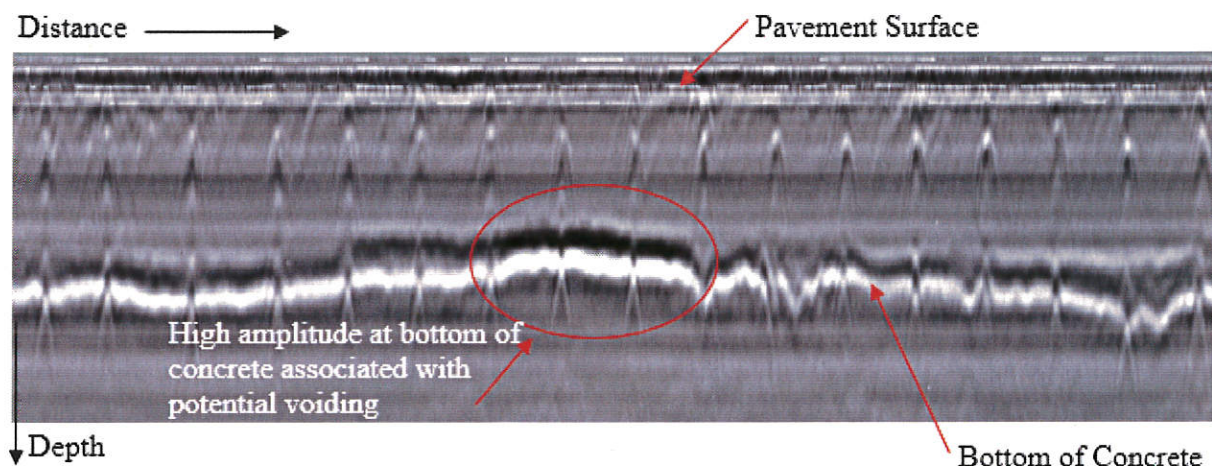


Figure 8 - Example GPR Analysis showing variations in layer elevation.

4. Survey Results

At the completion of the work, a project summary report will be provided. The report will describe the IR and GPR equipment, the data collection and analysis procedures used to generate the final results, and the resulting condition maps and quantities. The infrared survey will result in delamination/ debonding quantities and maps (see Figure 9), the GPR survey will result in quantities and maps of potential areas of voiding (see Figure 10). Both the infrared and GPR maps will be included in the report in “pdf” format. If desired, they can also be provided as CADD compatible “dxf” files.

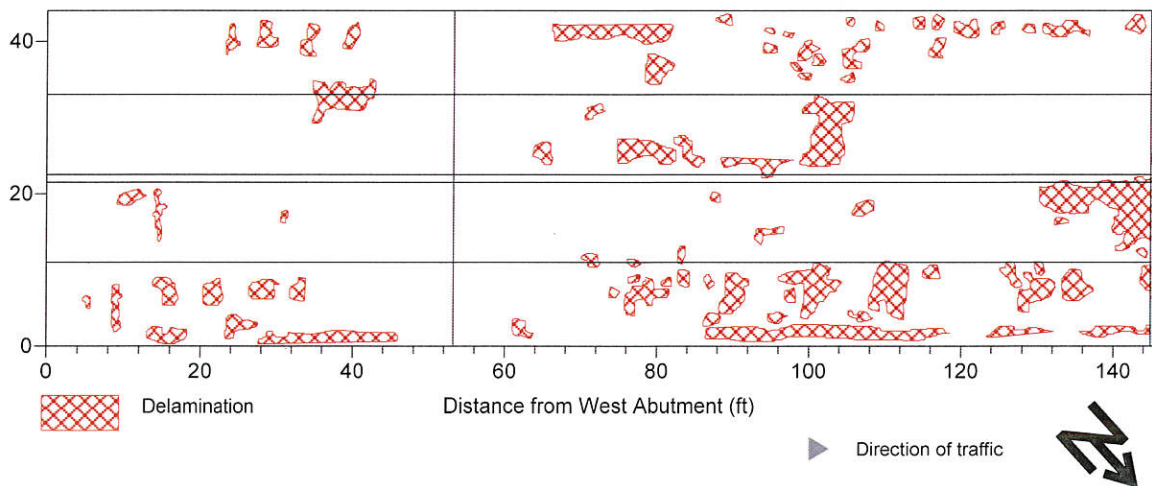


Figure 9 – Sample IR Survey Results

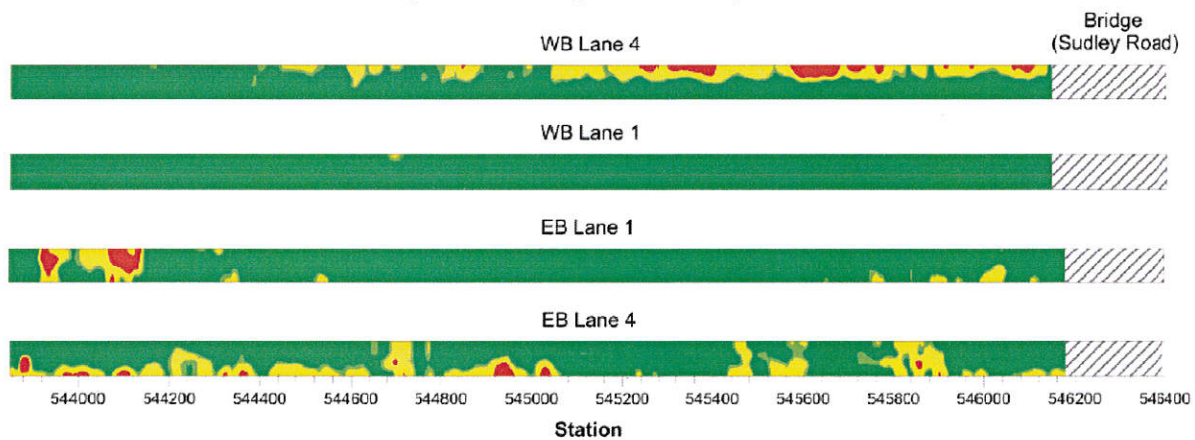


Figure 10 – Sample GPR Survey Results – Void Map

Infrasense recommends that the La Crosse DOT qualify the GPR void survey results by taking core samples at locations that the GPR indicates a high probability of voiding, as well as a few locations that are low risk and no risk as identified in the final maps.

5. Cost

The lump sum cost to perform the work outlined within this proposal is \$23,896. The breakdown of the lump sum cost by each major task is provided below.

	Task Totals
Mobilization	\$6,111
Data Collection*	\$4,797
Data Analysis	\$11,093
Report	\$1,895
	\$23,896

*Our budget assumes the City of La Crosse will provide traffic control for this work. This will include a TMA for the rolling closures required to complete the deck delamination surveys.

Modified STANDARD TERMS AND CONDITIONS

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2. **STANDARD OF PERFORMANCE.** Contracting Party agrees that the performance of the services, pursuant to the terms and conditions of this Agreement, shall be performed in a manner consistent with the degree of care and skill ordinarily exercised by members of the same professions currently practicing under similar circumstances providing like services. Contracting Party agrees to abide by all applicable federal, state and local laws, regulations and ordinances, and all provisions of this Agreement.
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14. **INSURANCE.** Contracting Party shall, at its sole expense, obtain and maintain in effect at all times during this Agreement the following insurance coverage:
 - 1) Commercial General Liability Insurance of not less than \$1,000,000.00 per occurrence for bodily injury, personal injury and property damage;
 - 2) Automobile Liability Insurance of not less than \$1,000,000.00 per occurrence for bodily injury and property damage covering all vehicles to be used in relationship to this Agreement;
 - 3) Umbrella Liability Insurance of not less than \$1,000,000.00 per occurrence for bodily injury, personal injury and property damage in excess of coverage carried for commercial general liability and automobile liability;
 - 4) Professional Liability Insurance of not less than \$1,000,000.00 per claim and annual aggregate; and
 - 5) To the extent that Contracting Party employs any employees or as otherwise required by law, Workers' Compensation and Employees' Liability Insurance with Wisconsin statutory limits.On the certificate of insurance, La Crosse shall be named as an additional insured on any General Liability Insurance, Automobile Insurance, and Umbrella Liability Insurance. The certificate must state the following: The City of La Crosse, its officers, agents, employees, and authorized volunteers shall be Additional Insureds. Prior to execution of the Agreement, Contracting Party shall file with La Crosse, a certificate of insurance signed by the insurer's representative evidencing the coverage required by this Agreement. Such evidence shall include an additional insured endorsement signed by the insurer's representative. Contracting Party shall provide La Crosse with a thirty (30) day notice prior to termination or cancellation of the policy. La Crosse reserves the right to require review and approval of the actual policy of insurance before it executes this Agreement.
15. **INDEMNIFICATION.** To the fullest extent allowable by law, Contracting Party hereby indemnifies and shall defend and hold harmless, at Contracting Party's expense, La Crosse, its elected and appointed officials, committee members, officers, employees or authorized representatives or volunteers, from and against any and all suits, actions, legal or administrative proceedings, claims, demands, damages, liabilities, losses, interest, attorney's fees (including in-house counsel legal fees), costs and expenses ~~of whatsoever kind, character or nature whether arising before, during, or after completion of the Agreement hereunder and in any manner directly or indirectly caused or contributed to in whole or in part, by reason of any act, omission, fault, or negligence, whether active or passive of Contracting Party, or of anyone acting under its direction or control on its behalf in connection with or incident to the performance of this Agreement, regardless if liability without fault is sought with or imposed on La Crosse.~~ Contracting Party's aforesaid indemnity and hold harmless agreement shall not be applicable to any liability caused by the willful misconduct of La Crosse, its elected and appointed officials, officers, employees or authorized representatives or volunteers. Nothing in this Agreement shall be construed as La Crosse waiving its statutory limitation and/or immunities as set forth in the applicable Wisconsin Statutes or other applicable law. This indemnity provision shall survive the termination or expiration of this Agreement.

from

negligent act

Contracting Party shall reimburse La Crosse, its elected and appointed officials, officers, employees or authorized representatives or volunteers for any and all legal expenses and costs incurred by each of them in connection therewith or in enforcing the indemnity herein provided. ~~Contracting Party's obligation to indemnify shall not be restricted to insurance proceeds, if any, received by La Crosse, its elected and appointed officials, officers, employees or authorized representatives or volunteers.~~
16. **NO PERSONAL LIABILITY.** Under no circumstances shall any trustee, officer, official, commissioner, director, member, partner or employee of La Crosse have any personal liability arising out of this Agreement, and Contracting Party shall not seek or claim any such personal liability.
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 - (3) Notify La Crosse, and provide copies, immediately, upon receipt, of any notice, pleading, citation, indictment, complaint, order or decree from any federal, state or local government agency or regulatory body, asserting or alleging a circumstance or condition that requires or may require a financial contribution by Contracting Party or any guarantor or an investigation, clean-up, removal, remedial action or other response by or on the part of Contracting Party or any guarantor under any environmental laws, rules, regulations, ordinances or which seeks damages or civil, criminal or punitive penalties from or against Contracting Party or any guarantor for an alleged violation of any environmental laws, rules, regulations or ordinances.
21. **SEVERABILITY.** The provisions of this Agreement are severable. If any provision or part of this Agreement or the application thereof to any person or circumstance shall be held by a court of competent jurisdiction to be invalid or unconstitutional for any reason, the remainder of this Agreement and the application of such provision or part thereof to other persons or circumstances shall not be affected thereby.

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 - (1) As soon as possible and in any event within a reasonable period of time after the occurrence of any default, notify La Crosse in writing of such default and set forth the details thereof and the action which is being taken or proposed to be taken by Contracting Party with respect thereto.
 - (2) Promptly notify La Crosse of the commencement of any litigation or administrative proceeding that would cause any representation and warranty of Contracting Party contained in this Agreement to be untrue.
 - (3) Notify La Crosse, and provide copies, immediately, upon receipt, of any notice, pleading, citation, indictment, complaint, order or decree from any federal, state or local government agency or regulatory body, asserting or alleging a circumstance or condition that requires or may require a financial contribution by Contracting Party or any guarantor or an investigation, clean-up, removal, remedial action or other response by or on the part of Contracting Party or any guarantor under any environmental laws, rules, regulations, ordinances or which seeks damages or civil, criminal or punitive penalties from or against Contracting Party or any guarantor for an alleged violation of any environmental laws, rules, regulations or ordinances.
 21. **SEVERABILITY.** The provisions of this Agreement are severable. If any provision or part of this Agreement or the application thereof to any person or circumstance shall be held by a court of competent jurisdiction to be invalid or unconstitutional for any reason, the remainder of this Agreement and the application of such provision or part thereof to other persons or circumstances shall not be affected thereby.