



City of La Crosse, Wisconsin

City Hall
400 La Crosse Street
La Crosse, WI 54601

Meeting Agenda

Heritage Preservation Commission

Thursday, July 3, 2025

3:30 PM

Miller Room

Special Meeting

Members of the public may participate in the meeting in the following ways:
View Virtually and Speak:

Join Zoom Meeting

<https://cityoflacrosse-org.zoom.us/j/84101189486?pwd=bFhXeHBwdWR3YkhOcmpzWWYrTjRvQT09>

Meeting ID: 841 0118 9486

Passcode: 810151

Phone Only

1 312 626 6799

Or you may attend in person at City Hall located at 400 La Crosse Street. Members of the public who would like to provide written comments on any agenda may do so by emailing acklint@cityoflacrosse.org, using a drop box outside of City Hall or mailing the Department of Planning and Development, 400 La Crosse Street, La Crosse WI 54601. Questions, call 608-789-7512.

Call to Order

Roll Call

Approval of Minutes

1. Approval of the May 22, 2025 Meeting Minutes.

Agenda Items:

2. [25-0743](#) Review of Certificate of Appropriateness for the property located at 327 Jay Street. (Downtown District)

Attachments: [Staff Report](#)

[Side Elevation](#)

[Building Permit App & Plans](#)

[Kevin Kellogg Email.pdf](#)

[Kevin Kellogg Example 1](#)

[Kevin Kellogg Example 2](#)

[Kevin Kellogg Example 3](#)

[Kevin Kellogg Example 4](#)

[Kevin Kellogg Example 5](#)

[Kevin Kellogg Example 6](#)

[Railing Examples 7-3-25.pdf](#)

Adjournment

Notice is further given that members of other governmental bodies may be present at the above scheduled meeting to gather information about a subject over which they have decision-making responsibility.

NOTICE TO PERSONS WITH A DISABILITY

Requests from persons with a disability who need assistance to participate in this meeting should call the City Clerk's office at (608) 789-7510 or send an email to ADAcityclerk@cityoflacrosse.org, with as much advance notice as possible.

Heritage Preservation Commission Members

CM Mackenzie Mindel, Laura Godden, Natalie Heneghan, Jim Gallagher, Ellie McLoone, John Reiman, Bruce Banes



City of La Crosse, Wisconsin

City Hall
400 La Crosse Street
La Crosse, WI 54601

Text File

File Number: 25-0743

Agenda Date: 6/26/2025

Version: 1

Status: Agenda Ready

In Control: Heritage Preservation Commission

File Type: Review of Plans

Agenda Number: 2.

HERITAGE PRESERVATION COMMISSION REPORT
Certificate of Appropriateness- Downtown Commercial Historic District.
Section 115-320(d)(2)

TO: Heritage Preservation Commission
FROM: Planning Staff
MEETING DATE: June 26, 2025

PROPOSAL: The applicant is proposing to install a deck on the rear façade of the building located at 327 Jay Street.

PROPERTY/BUSINESS OWNER:
KLC Properties LLC
PO Box 1534
La Crosse, WI 54602

APPLICANT:
Brent Thielen
Steiger Construction

BACKGROUND: The Downtown Commercial Historic District was established by Ordinance in April 2016 along with design standards that are specific to the styles of architecture found within. The local district designation of the Downtown Commercial Historic District was created to encourage preservation of the historic resources of the district, which was named to the National Register of Historic Places in 1994. The design standards and review procedures are used to guide preservation, rehabilitation, new construction, relocation and demolitions. Plans or requests must be approved by the Heritage Preservation Commission prior to any building/demolition permit being issued. In determining whether to issue a Certificate of Appropriateness for Demolition, the HPC shall:

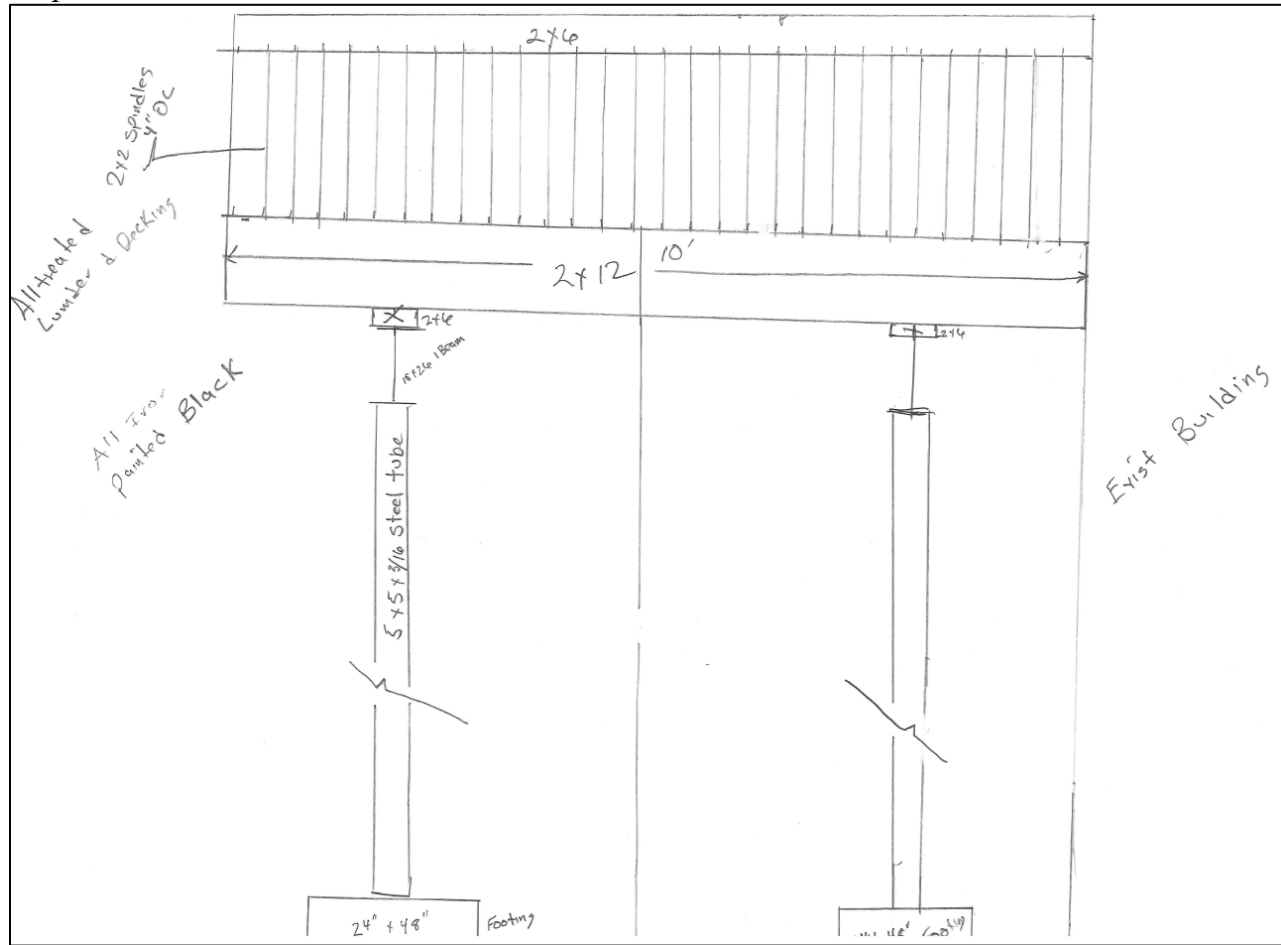
1. Consider factors such as whether the structure is of historical significance, the state of repair of the building, and whether denial of a permit would create a significant economic hardship for the owner.
2. Issue the certificate only if the owner can demonstrate that the building is structurally unsound and/or that denial of a demolition permit would create a significant economic hardship for the owner.

PROJECT DESCRIPTION: The applicant is proposing to install a 10' x 18' deck on the rear faced of the property located at 327 Jay Street. (Pickermans) The deck is intended to serve the second story apartment. The top of deck platform is approximately 12' 6" from grade and supported by four steel tubes. The deck platform is comprised of treated lumber and steel I-beams. The railing and spindles are also treated lumber. The spindles are 2"x 2" in size with no additional ornamentation. The proposed deck will not be attached to the building as the brick is too soft. It will be built flush up to the building.

Project Location



Proposed Plan



SEE ATTACHED FOR MORE INFORMATION

ANALYSIS:

The relevant design standards to this project are: (There are few that directly correlate to this type of project)

(f) New Construction

(4) Building Materials

(a) Building materials shall be consistent with the predominant materials in use in the District.

(e) Additional materials may be appropriate on side or rear facades that are less visible from the street, where permitted by the HPC. Materials that may be considered include:

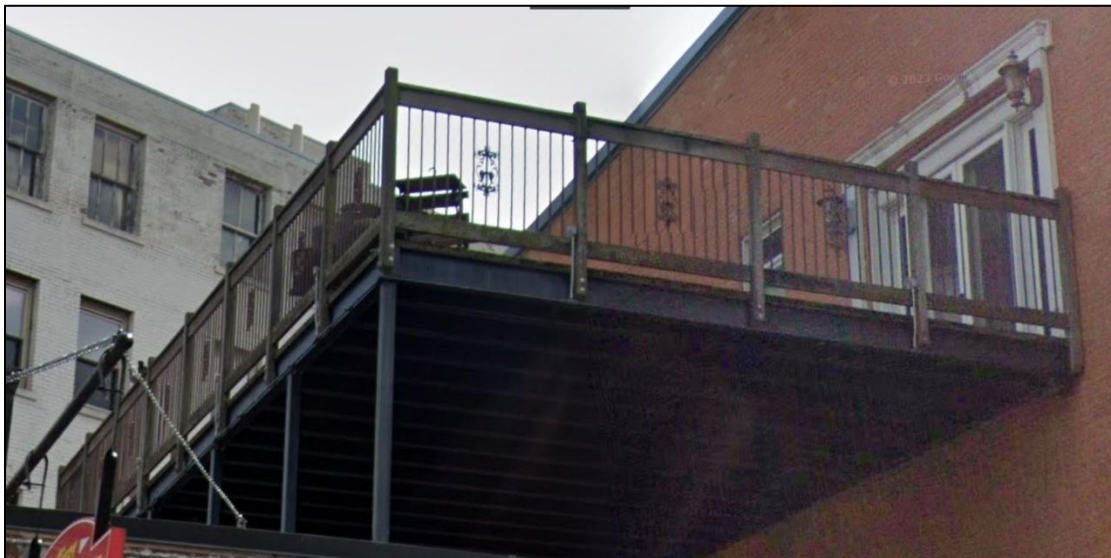
1. Precast concrete units and concrete block, provided that surfaces are molded, serrated or treated with a textured material in order to give the wall surface a three-dimensional character.
2. "Jumbo brick" units shall only be used on the lower third of the building wall.
3. Split-faced brick or block is not acceptable for downtown buildings.

4. "Novabrick" may be acceptable in some uses.

(6) *Accessory buildings.*

(a) Design of accessory buildings should be simple and unobtrusive. Materials similar to those of the principal building are encouraged, but lower cost materials may also be appropriate as determined by the HPC.

The above standards were intended to be applied more so to buildings instead of decks but are the most relevant. When evaluating its consistency with predominant materials in the district staff looked at what other decks have been added. There are very few.



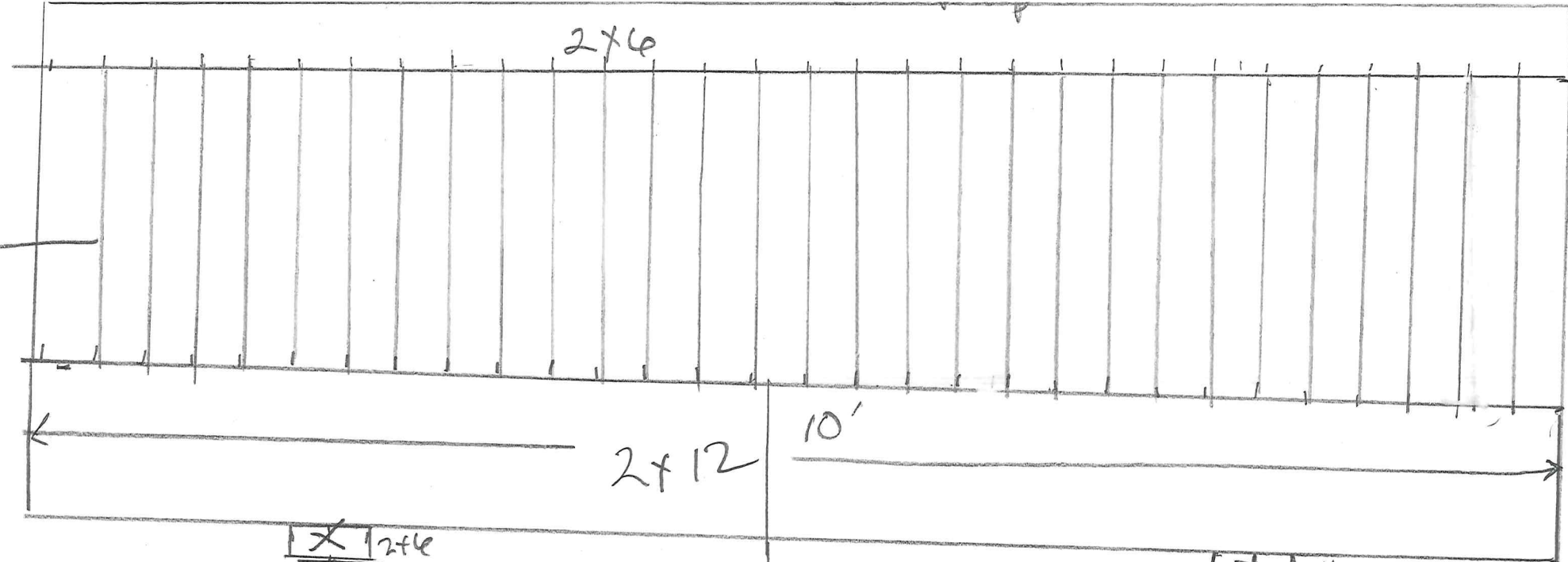
Whole both of these examples use wood in their design, there is some inclusion of wrought iron or metal components, particularly with the railing.

FINDING: The proposed deck is at the rear of the building where there is typically more flexibility in design and use of materials. However, due to its location and high use of the adjacent alley, it will be highly visible, even from Jay Street. The design standards for the district are not particularly clear when reviewing these types of projects so staff looked at other decks that were added. All of them have a mix of visible wood and ornamental metal, especially as part of the railings. This design element was proposed to the applicant, but they chose to submit it as is. The proposed design is, in staff's opinion, not consistent with other decks and materials but the design standards are not crystal clear on this.

RECOMMENDED ACTION BY STAFF: This Certificate of Appropriateness is recommended for approval with the condition some ornamental metal is worked into the design of the railing.

All treated
Lumber & Decking
2x2 Spudles
4" OC

All Iron
Painted Black



2x6
18' 2x6 Beam

2x6

5 x 5 x 3/16 Steel tube

24" x 48" Footing

24" x 48" Footing

Exist Building

Department of Planning and Development

Building and Inspections

inspection@cityoflacrosse.org 608-789-7530

APPLICATION FOR RESIDENTIAL BUILDING PERMIT

Application Number: _____ Date: _____ Parcel Number: _____

OWNER INFORMATION

Name: <u>KLC Properties</u>			
Address of above: Street <u>PO Box</u>	City <u>1534 LaCrosse</u>	State <u>WI</u>	Zip Code <u>54602</u>
Phone:	Cell:	Email:	

CONTRACTOR INFORMATION

Name: <u>Steiger Construction</u>			
Address of above: Street <u>2812 28th St. S.</u>	City <u>LaCrosse</u>	State <u>WI</u>	Zip Code <u>54601</u>
Phone: <u>608-788-4233</u>	Cell: <u>608-790-5289</u>	Email: <u>brent@steigerconstruction.com</u>	

PROJECT INFORMATION

Project address: <u>327 Jay St</u>		Description of Work If Demolition, include use of land after demolition <u>Free standing Deck</u> <u>10' x 18' Behind Building</u> <u>Approx. 14' Above grade</u>
Construction Cost: \$ <u>22,000</u>	Fence Only: Height: _____ Material: _____	
Project Type: Building <input type="checkbox"/> Addition <input type="checkbox"/> Sign <input type="checkbox"/> Alteration/Remodel <input type="checkbox"/> Demolition <input type="checkbox"/> Accessory <input checked="" type="checkbox"/>		

PROPERTY INFORMATION

Zoning	Nbr. Dwelling Units	Owner Occ <input type="checkbox"/> Rental <input type="checkbox"/>	Airport Height Yes <input type="checkbox"/> No <input type="checkbox"/>	Flood Plain Yes <input type="checkbox"/> No <input type="checkbox"/>	Fire Limits Yes <input type="checkbox"/> No <input type="checkbox"/>	Archaeological District Yes <input type="checkbox"/> No <input type="checkbox"/>	Historical Dist Yes <input type="checkbox"/> No <input type="checkbox"/>
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FEE INFORMATION

Plan Review: \$	Permit: \$	Record Mtce: \$	Expedited: \$	Started w/o permit: \$	Exempt: <input type="checkbox"/>	Other: \$	Total: \$
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IT IS HEREBY AGREED between the applicant, as owner, owner's agent or servant, and the City of La Crosse that for and in consideration for the premises and of the permit to construct, erect, alter, move, raze, or install and the occupancy of a building adding or property as above described, to be issued and granted by Building and Inspections of the City of La Crosse, that the work thereon will be done in accordance with the descriptions set forth in this statement, and as more fully described in the City of La Crosse and State of Wisconsin laws relating to the construction, alteration, repairs, removal and safety buildings and other structures and permanent building equipment.

Steiger Construction Agent/Contractor: (Print) Brent W. Steiger (Sign) 4-15-25 (Date)

-DCQ

-DC

Expires ____ / ____ / ____

Expires ____ / ____ / ____

Owner: (Print)

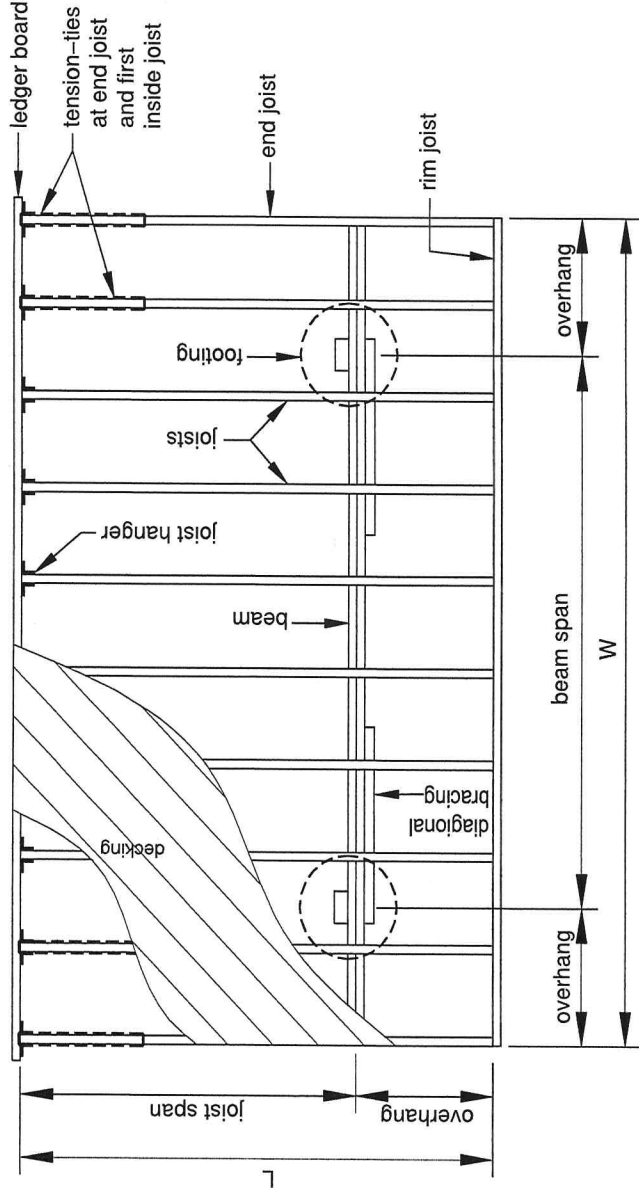
(Sign)

(Date)

OFFICE USE ONLY

Application Approved:	Inspector:	Date:
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TYPICAL DECK FRAMING PLAN



Decking: ☐ 2x4 ☐ 2x6 ☒ five-quarter board ☐ wood-plastic composite (per ASTM D 7032)
☐ Other decking, evaluation report number: _____

Joists: size: ☐ 2x6 ☐ 2x8 ☐ 2x10 ☒ 2x12 spacing: ☐ 12 in. ☒ 16 in. ☐ 24 in.
 joist span dimension: 10' ft. - 0 in. overhang dimension: 1 ft. - 0 in.

overhang: ☒ Yes ☐ No rim joist: ☐ 2x6 ☐ 2x8 ☐ 2x10 ☒ 2x12

Beam(s): number of plies: ☐ 2 ☐ 3 size: ☐ 2x6 ☐ 2x8 ☐ 2x10 ☐ 2x12
 overhang: ☐ Yes ☐ No overhang dimension: _____ ft. - _____ in.

Posts: size: ☐ 4x4 ☐ 4x6 ☐ 6x6 height: _____ ft. - _____ in.
 size: _____ in. ☐ square ☐ round thickness: _____ in.

Footings: ledger board size: ☐ 2x8 ☐ 2x10 ☒ 2x12 ☒ Not applicable (free-standing deck)
 fastener: ☐ Through bolt ☐ Lag screw ☐ Wood screw

Lateral support: ☐ Expansion anchor ☐ Adhesive anchor ☐ Diagonal bracing, size: ☐ 2x
☐ Tension-tie

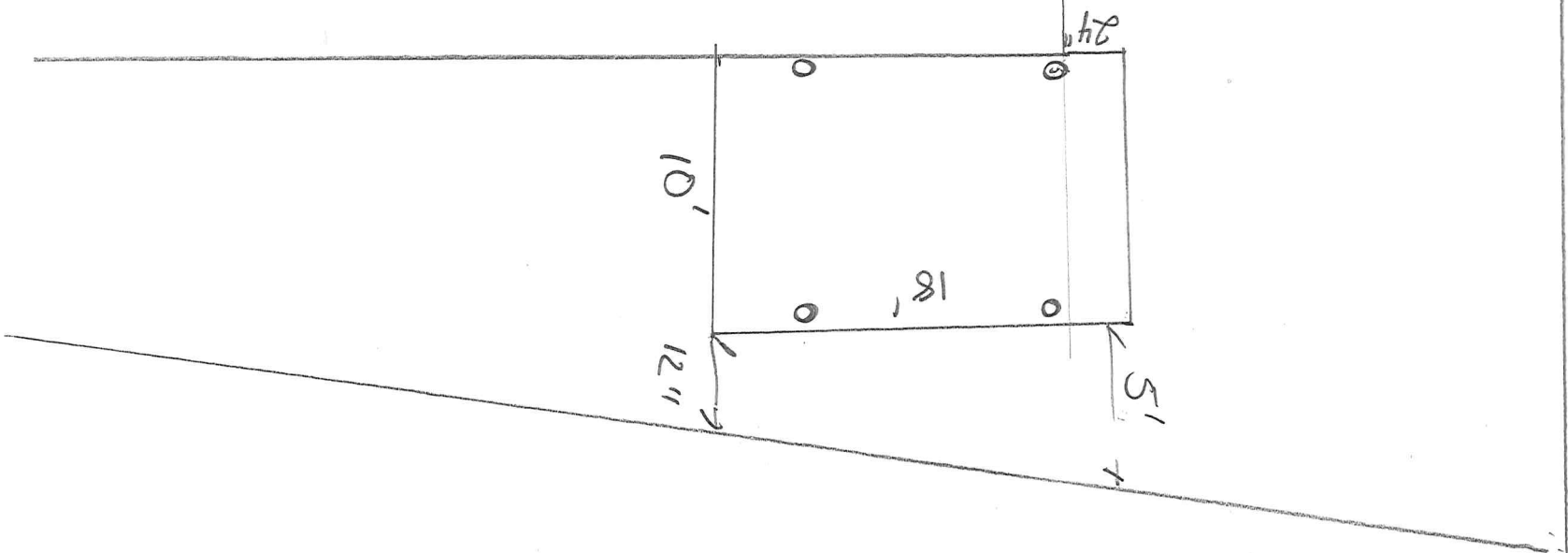
(not permitted for free-standing deck)

Deck size: L = 18 ft. - 0 in. W = 10 ft. - 0 in.

Guards: "Height" - Guards shall extend to at least 36 inches above the floor or to the underside of a stairhandrail complying with s.SPS 321.04 (3) (b). Measurement shall be taken from the hard structural surface beneath any finish material to the top of the guard.

"Opening size" - Guards shall be constructed to prevent the through-passage of a sphere with a diameter of 4 3/8 inches, when applying a force of 4 pounds.

N
Not to Scale



327 Jax St

Sec. 103-98. Construction within the fire limits.

(a) No building or structure of wood frame or ordinary construction shall be erected or increased in height, no building or structure shall be extended on any side by wood frame or ordinary construction, nor shall wood or other combustible veneers be permitted within the fire limits, with the following exceptions:

- (1) Wood or other combustible veneers on noncombustible backing for show windows that do not extend above the first full story above grade.
- (2) A building occupied as a private garage, not more than one story in height nor more than 750 square feet in area, located on the same lot with a dwelling; provided that such building shall be placed at least two feet from the lot lines of adjoining property.
- (3) Buildings or ordinary construction, except when used for a high hazard occupancy, not exceeding 2,500 square feet in area when used for a business occupancy or 1,000 square feet in area when used for other occupancies, nor more than one story in height, and having a horizontal separation of not less than ten feet on all sides. Walls having a horizontal separation of less than ten feet shall have a fire resistance rating of not less than one hour.
- (4) Enclosed and open air parking garages of ordinary construction.
- (5) Greenhouses not more than 15 feet in height erected on the same lot with and accessory to a dwelling or a store.
- (6) Sheds open on the long side, not more than 15 feet in height nor more than 500 square feet in area, located at least five feet from buildings and from adjoining lot lines.
- (7) Builders' shanties for temporary use in connection with a building operation and located on the same lot with such building operation, on a lot immediately adjoining, on an upper floor of the building under construction, or on a sidewalk.
- (8) Piazzas, balconies, fire exiting stairs and platforms, decks (not including closed-in porches) on buildings not exceeding ten feet in width, provided that no such structure shall be located nearer than three feet to an adjoining lot line or be joined to a similar structure or another building. Existing structures of wood frame or ordinary construction may be repaired or replaced to existing dimensions with fire resistant or pressure-treated lumber providing that a building permit is obtained from the

Department. Existing noncombustible structures may not be repaired or replaced with combustible materials.

(9) Fences not exceeding ten feet in height.

(10) Display signs when in compliance with all other laws of the City.

(11) Cooling towers.

(12) Roofs over parking lots and bus stations of ordinary construction, where the roof is at least ten feet above the floor, and every 40 feet there is an open roof ventilation area six feet wide extending either the full length of the roof or the full width of the roof.

(13) Boat storage buildings located on water.

(14) Any building with full masonry exterior veneer on the first floor with non-combustible or equivalent coverings on the remaining floors exclusive of windows, doors and decorative trim, and fully protected by a NFPA 13 automatic fire sprinkler system with continuous off-site water flow monitoring, and subject to approval of the Fire Department - Division of Fire Prevention and Building Safety.

 (15) The Fire Chief may, at their discretion, on a case-by-case basis, deviate from the requirements in this Section.

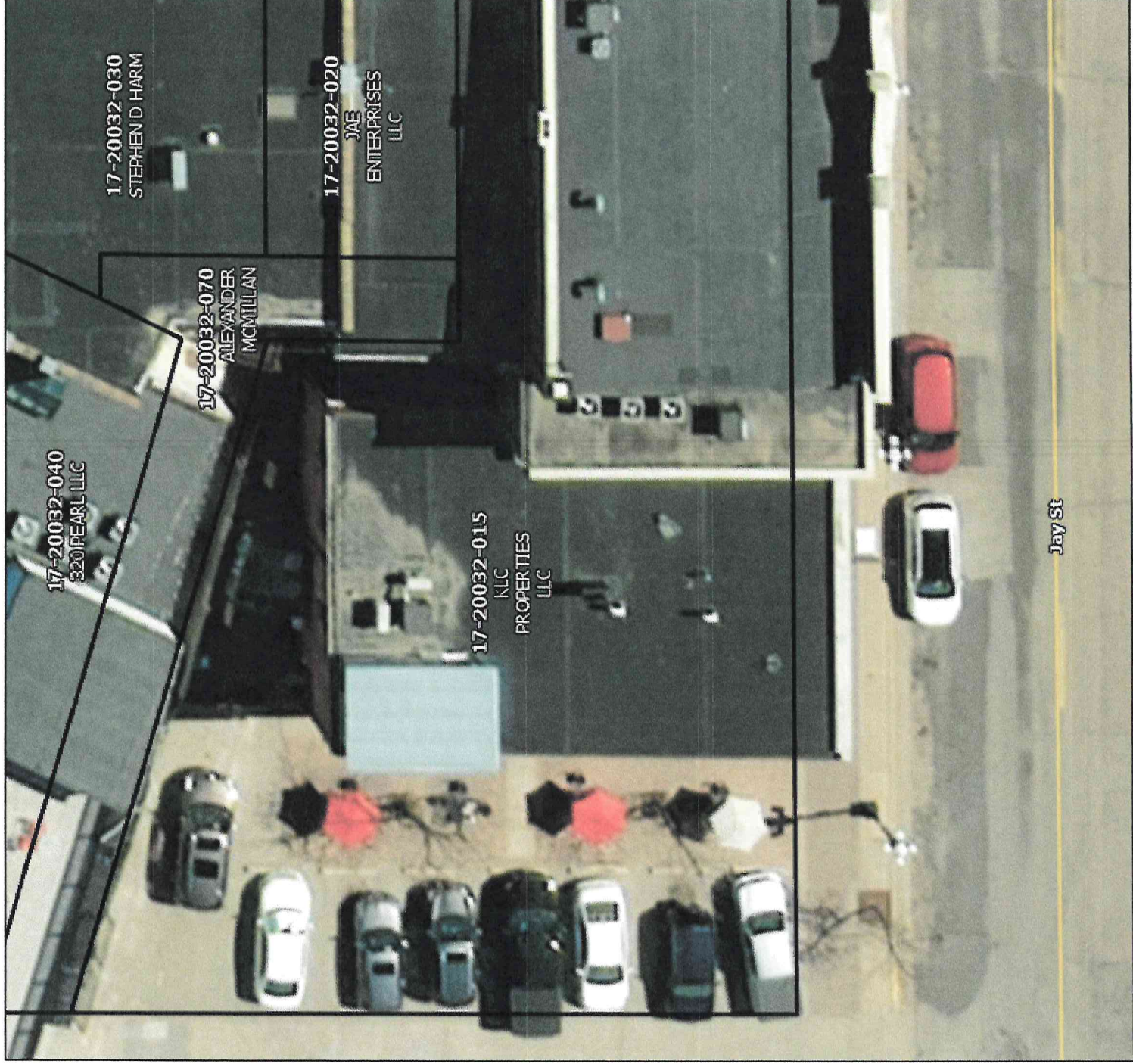
(b) No building or structure of wood frame construction or ordinary construction shall be moved into or within the fire limits.

(c) A building or structure shall be deemed to be within the fire limits if one-third or more of the area of such building or structure is located therein.

(Code 1980, § 16.09(B); Ord. No. 4911, § I(attach.), 1-14-2016; Ord. No. 4974, § II, 3-9-2017; Ord. No. 5261, § IX, 9-18-2023)

Cross reference(s)—Fire limits, § 103-4.

ArcGIS Web Map



4/15/2025, 2:20:58 PM

Municipality Limits Labels

Municipality Limits

Road Centerlines

Local Road

Property Owners with Assessments

Representative Fraction (RF) or Natural Scale: 1:1200 (this is the same as 1/1200) The RF says that 1 of any measurement on the map equals 1200 of the same measurement on the original surface;

La Crosse County WI Zoning Planning and Land Information Department
212 6th St N, Suite 1300
La Crosse, WI 54601
<https://lacssecounty.org/zoning>
608-785-9722

Web AppBuilder for ArcGIS

Acklin, Tim

From: kbk sparks <kbksparks@yahoo.com>
Sent: Tuesday, June 24, 2025 8:56 PM
To: Acklin, Tim
Subject: HPC Meeting for KLC / Wiggerts Deck
Attachments: IMG_6040.PNG; IMG_6038.PNG; IMG_6037.PNG; IMG_6036.PNG; IMG_6035.PNG; IMG_6033.PNG

You don't often get email from kbksparks@yahoo.com. [Learn why this is important](#)

*** **CAUTION:** This email originated from an external sender. **DO NOT** click links or open attachments unless you recognize the sender and know the content is safe. ***

Hello Tim,

I continue to help the Wiggert family with their buildings regularly and requested Brent and Steigers involvement with this deck project as the contractor.

I see the recommendation to approve with railing modifications and there is no concern from the Wiggerts to make a change to the railing design. As you pointed out there also really is no preferred design standard either for decks or railings. It also appears you may have suggested that to Brent however that was not clearly conveyed back to the Wiggerts or Myself. The Wiggerts have spent many many dollars on their downtown buildings trying to improve appearances and a railing redesign is far from a major concern.

To add a little context to this proposal / permit application process. Brent spent 30+ days going in circles with the Inspection department on this deck application with them continually having a new reason it wasn't allowed in which none were true, but it seems someone has his own way to read the fire district ordinance other than the way it is written. After inspection had no more reasons to deny it Brent was then told ohh and by the way it has to go to HPC and that meeting was last night meaning another 30-day delay. So, by the time this made it to your desk Brent may have been worn out with new suggestions such as your railing ideas.

I can also say Brent and myself looked at several downtown decks at the start of this process and there are several with all wood railing and no metal involved. I am not sure why the HPC agenda seems to indicate those types of decks and railing do not exist in the downtown. The agenda clearly states, "All of them have a mix of visible wood and ornamental metal". I have attached several pictures for reference and would appreciate if we could send them out to the commission's membership with some clarification to the agenda's "findings" as those findings seem inaccurate when reviewing the attached pictures.

Picture 6040 is behind 531 Main St and has an all-wood railing

Picture 6038 is Phil Addis past residence and does have some steel spindles, but they are not really ornamental

Picture 6036 is part of Bell Square and has an all-wood railing on the predominant side directly on the city sidewalk

Picture 6035 is behind Noelke's store on Main St and is all wood. Railing, framing, steps, everything is wood.

Picture 6033 faces the State Bank lot from 425 Main St when James remodeled that property.

Picture 6037 may or may not be relevant but is from a building across La Crosse Street from city hall

While We are willing to alter the railing design, I'm just asking for this application to be fairly represented as there are several clear examples of all wood railing designs in downtown which originally led us to feel it must have been an ok option. Please just steer us in a preferred design style.

Thank you for your time on this project Tim.
Kevin Kellogg

Sent from my iPhone



Crop





Crop

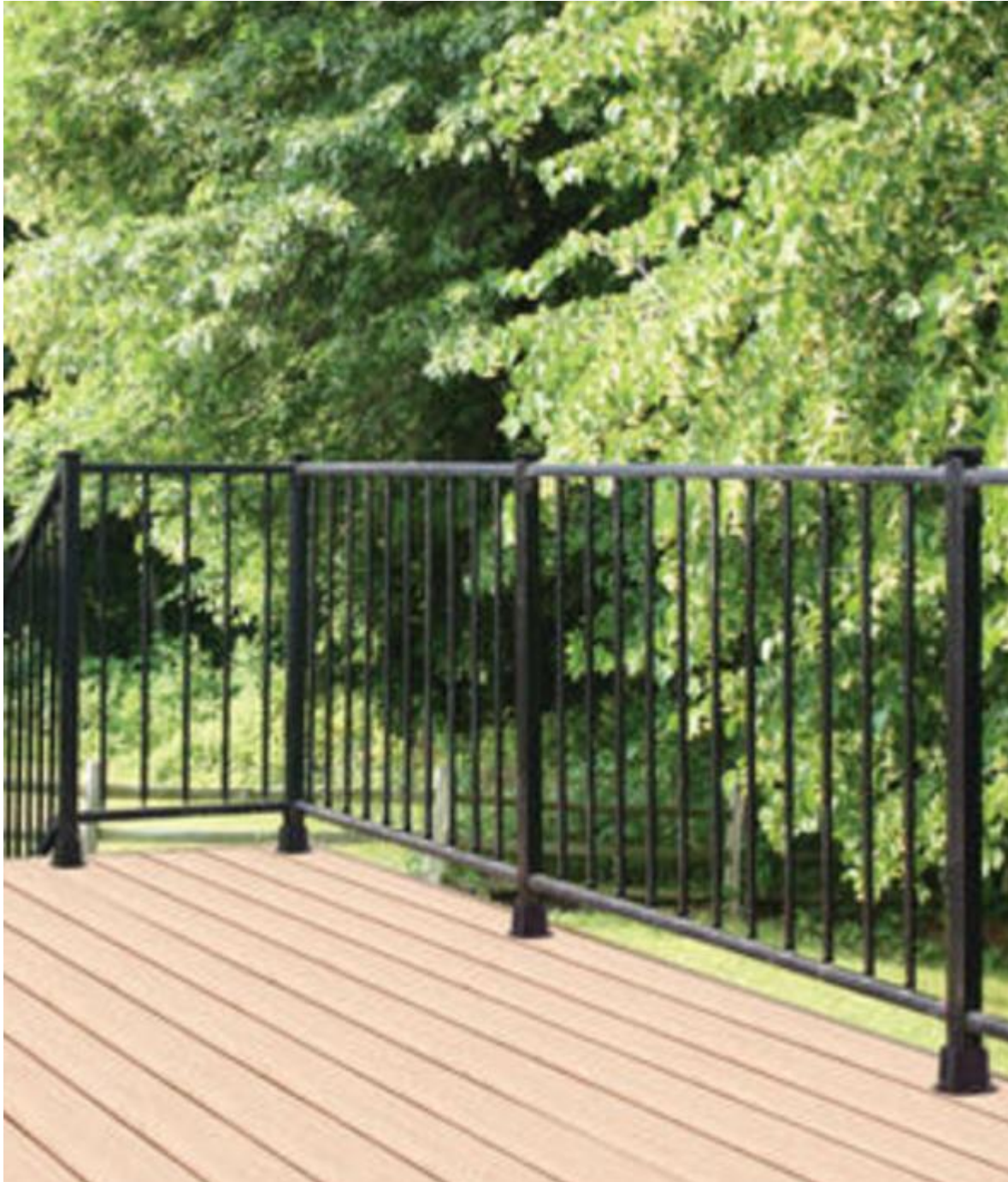


Crop





Example 1



Example 2

